

THE SIDINGS

SITE SPECIFIC GUIDELINES

(LOTS 669, 500, 499, 502 & 503)
YELVERTON DRIVE, MIDLAND

Adopted by Midland Redevelopment Authority
23 February 2010



THE SIDINGS (LOTS 669, 500, 499, 502 & 503) YELVERTON DRIVE, MIDLAND

INTRODUCTION

These guidelines apply to The Sidings within the Helena Precinct of the former Midland Railway Workshops, bounded by the railway line to the north, rail reserve to the west, Centennial Place to east and Yelverton Drive to the south as outlined on the plan shown below.

These guidelines have been prepared and adopted pursuant to Part Five of the Midland Redevelopment Scheme.

The Sidings are directly north of the Workshops, a proposed mixed-use area comprising education, commercial, residential and creative industries activities. To the north is the traditional town centre of Midland and directly northwest is the Midland train and bus station. To the west is the new residential community of Woodbridge overlooking the Coal Dam Park. To the east are a mix of government and non-government activities, including major WA Police services, the future Midland Health Campus and commercial and bulky goods retail.

The site is under the control of the Midland Redevelopment Authority (the Authority), an agency specifically established to assist with the revitalisation of Midland. These guidelines are aimed at increasing certainty about future built form and assisting in expediting development approval.

HERITAGE CONSIDERATIONS

In June 2008 the former Midland Railway Workshops and its curtilage, including The Sidings, was entered on the permanent State Register of Heritage Places. Memorials on land titles reflect this status. All development applications will be referred by the Authority to the Heritage Council of Western Australia (HCWA) for comment prior to determination.

These guidelines have been developed in conjunction with the Heritage Council. In addition to the guidelines, applicants should refer to the "Heritage Strategy for the Midland Central Redevelopment Area" Nov 2001. The document provides additional guidance on responding to heritage within this area.



Administration Building Styles



Industrial Building Style

PLANNING INTENT AND OBJECTIVES

The planning intent is for The Sidings to continue to be used as a support area for the Workshops, containing car parking and built form that complements the Workshops buildings and respects the heritage significance of the precinct (refer to Design Intent Plan – Figure 1).

Priority will be given to pedestrian movement in this area, ensuring strategic pedestrian links between the Midland train station, the town centre, the Health Campus and future activities of the Workshops are enhanced. Helena Street, Centennial Place and Cale Street are regarded as priority pedestrian routes. An at-grade pedestrian crossing linking Centennial Place to Cale Street (Juniper Gardens and Midland Gate) is proposed.

The Sidings will provide a strategic parking area to service the Workshops and environs. Lot 502 has the capacity to accommodate a decked car park in the future, located to align with Block 1.

New built form will be developed at either end of The Sidings, providing a frame to a central view corridor and at-grade parking area.

Helena Street provides a vital link from the town centre to the Workshops. Views between these areas have been enhanced by creating small pocket parks either side of Helena Street, south of the rail line. New buildings will be developed beside these parks forming a 'gateway' to the Workshops.

At the western end of the precinct (Lots 499, 500 and 669) new buildings will complement the 'Administration' style buildings of the Midland Railway Workshops (e.g. Railway Institute and Chief Mechanical Engineer's Office) and new development at the eastern end (Lots 502 and 503) will complement the larger 'Industrial' style buildings (e.g. Block 1).

In addition to complementing the historical character of the area, new development will be required to:

- Be contemporary in design, however pay careful attention to the scale, form, rhythm and proportions of the heritage buildings
- Complement and enhance the network of pedestrian paths to encourage pedestrian movement linking transport to activities
- Ensure the protection of vistas and views from the rail reserve, through to the former shunting yard (future Railway Square) and the gardens of the Peace Memorial, and
- Allow for built form that promotes interest, security and safety through the activation of streets and other public areas.

Characteristics of the 'Administration' style buildings are:

- Simple rectilinear floor plans
- Large gable or hipped roofs
- Projecting awnings fixed over the bands of windows
- Warm face-brickwork contrasting with rendered horizontal banding
- Simple decorative ornament to the cornice and upper wall areas, and
- Regular patterns across the facades by the arrangement of openings and the repetition and vertical proportions of elements.

Characteristics of the 'Industrial' style buildings are:

- Robust and simple forms
- Flat or saw tooth roofs with parapet walls
- Stepped or corbelled parapet gables
- Warm face-brickwork
- Facades divided into regular vertical bays formed by piers and recessed arches
- Regular vertically proportioned openings, and
- Clerestory windows or roof lights.

Future development may include a decked car park. This will require a high standard of design with particular attention to the scale, rhythm and proportion of the building, it's interface with the street - especially at ground level - and its impact on the cultural heritage values of the Workshops.

An 'Industrial' style building is sought with its façade having a regular rhythm of vertical bays of a similar proportion to that of Block 1, adjusted to suit the spacing of car bays.

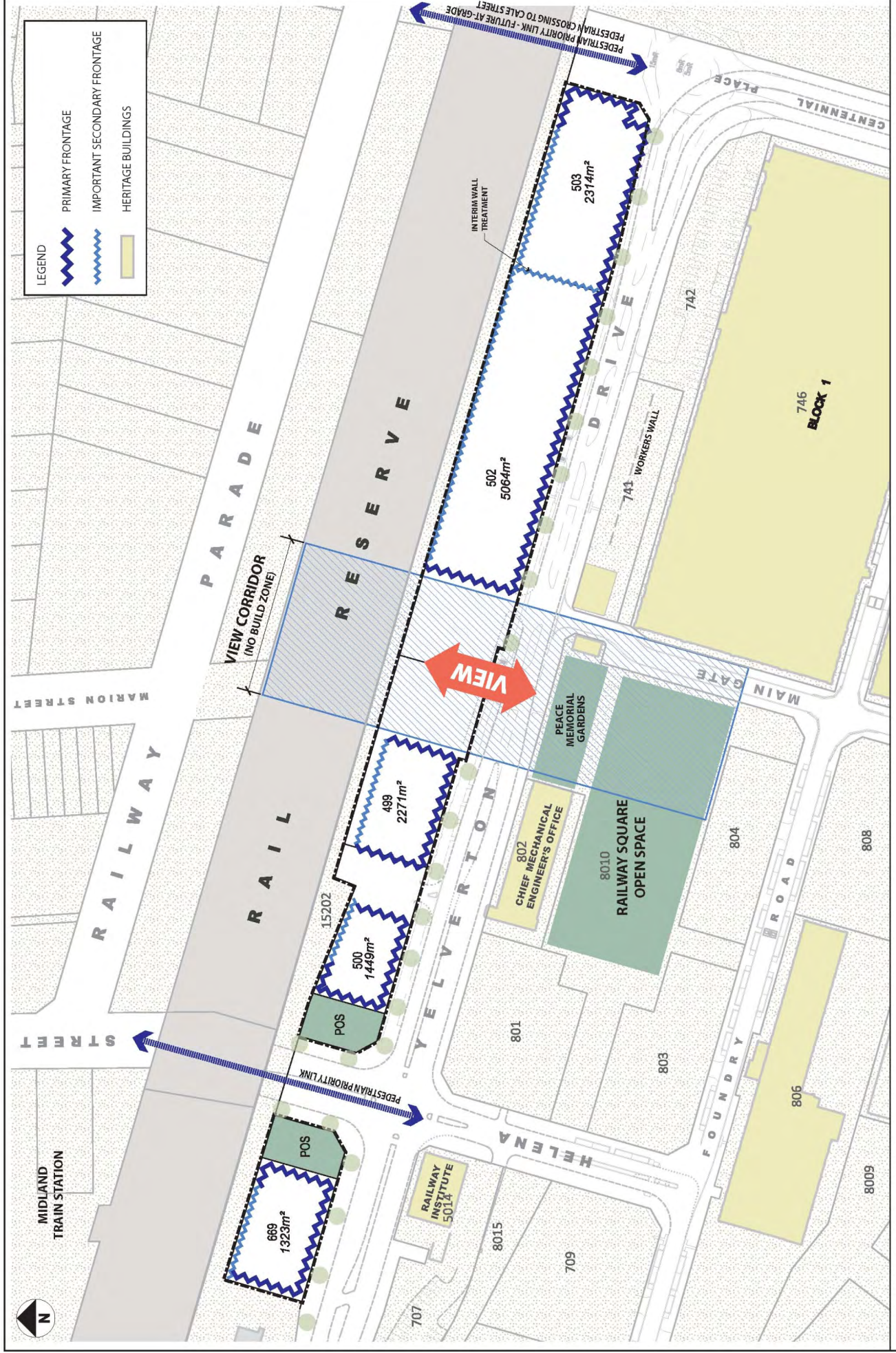
Active ground floor uses are sought to the street frontage and to the east end abutting the future pedestrian link. To maximise parking levels within the height limit, it is likely that the top level will not have a structured roof. Lightweight shading that is set back from the perimeter walls may be considered.

Integration of artistic elements that add richness and interest in a sympathetic manner are highly desirable. Use of a heritage architect with experience of contemporary design in heritage settings is recommended.

RELATIONSHIP TO REDEVELOPMENT SCHEME

The Midland Redevelopment Scheme ('the Scheme') outlines the statutory processes and controls for land within the Authority's jurisdiction. The Scheme provides standards for parking and outlines preferred and potential land uses. It outlines objectives in relation to character, heritage and mixed use development and the procedures to manage these issues.

These Site Specific Guidelines identify design expectations for development within The Sidings. The Authority has adopted these guidelines under Part 5 of the Scheme and they should be read in conjunction with the Scheme Text and other relevant planning policies and design guidelines. In determining any application for planning approval, the Authority will have due regard to all relevant documents.



THE SIDINGS
DESIGN INTENT PLAN - FIGURE 1

YELVERTON DRIVE
 Lots 669, 500, 499, 502 & 503



MIDLAND
TRAIN STATION

STREET

MARION STREET

RAILWAY

PARADE
RESERVE

VIEW CORRIDOR
(NO BUILD ZONE)

RAIL

15202

POS

500

1449m²

499

2271m²

VIEW

502

5064m²

503

2314m²

INTERIOR WALL
TREATMENT

741

WORKERS WALL

742

746
BLOCK 1

PEACE
MEMORIAL
GARDENS

802

CHIEF MECHANICAL
ENGINEER'S OFFICE

8010

RAILWAY SQUARE
OPEN SPACE

804

803

806

809

808

807

709

RAILWAY
INSTITUTE
5014

8015

707

HELENA

FOUNDRY
ROAD

MAIN GATE

CENTENNIAL
PLACE

PEDESTRIAN PRIORITY LINK - FUTURE AT-GRADE
 PEDESTRIAN CROSSING TO CALE STREET

PEDESTRIAN PRIORITY LINK

APPROVAL PROCESS

Development applications are to be lodged with the Authority and are to include the details outlined within the 'Application Checklist' (see www.mra.wa.gov.au/images/93-Development_Application_Checklist.pdf).

Proponents are not required to prepare a site analysis plan or shadow diagram.

It is strongly recommended that prospective landowners and/or their architect/designer approach the Authority at the earliest stage of design development to discuss proposals and facilitate the planning approval process. The Authority encourages the use of design professionals with experience in heritage environments, due to the unique development requirements and special qualities of the site.

The Authority may approve an application involving departures from the guidelines if, in its opinion, the application satisfies the stated guideline objectives.

ENVIRONMENTAL DESIGN

The Authority wishes to foster the highest possible standards of development and promote innovation in design and construction, especially in the areas of energy efficiency, ecological sustainability, Crime Prevention Through Environmental Design (CPTED) and building design to ameliorate the effects of noise and vibration.

By way of example, the Authority encourages the provision of rainwater tanks and use of solar PV panels. Development that achieves a 4 Star Green Star rating or higher under the Green Building Council Australia rating system ([http:// www.gbca.org.au/](http://www.gbca.org.au/)) is strongly encouraged. Applicants are required to report on Environmentally Sustainable Design (ESD) standards before planning approval will be granted.

Many requirements in these guidelines are aimed at meeting the needs of visitors, residents and pedestrians in a way that also reinforces safety. CPTED principles have been incorporated into the design guidelines to reinforce the safety and security of the area.

The Sidings is located directly adjacent to the rail reserve, where freight and interstate trains travel on a regular basis. The western end of The Sidings is also within the 20-25 ANEF band of aircraft noise impact from Perth Airport associated with a future runway. All lots within The Sidings have a memorial on their certificate of title advising prospective purchasers of the potential for rail noise and vibration and future aircraft noise impact. The site is also impacted by vehicle noise from Yelverton Drive.

New development will be required to specifically address noise and vibration at development approval and building licence stages, with actions implemented in the construction stages.



Design should maximise the environmental performance of the building

GUIDELINES

1 BUILDING ENVELOPE & SETBACKS

- 1.1 Development shall be a minimum of 2 storeys with Lot 503 a minimum of 3 storeys.
- 1.2 Development shall have a maximum height of 9.5m to the eaves, 10.5m to the top of any parapet wall, and 12m to the ridgeline of any roof.
- 1.3 Nil setback required to Yelverton Drive, Centennial Place and any abutting public open space. Minor setbacks are acceptable to maintain rectilinear building forms.
- 1.4 The northern frontage (adjacent to the rail line) of Lots 669, 500, 499 and 503 shall be set back a minimum of 1m, to avoid solid walls on the rail reserve and to allow for windows and façade details facing north.

2 BUILT FORM AND FINISHES

- 2.1 Any building facade that is visible from public view shall be articulated as if it were the frontage of the building. The Design Intent Plan identifies primary frontages and important secondary frontages as a guide to their relative importance and level of detailing that is expected.
- 2.2 Façade treatments, including windows, awnings and other design articulation, are required to the important secondary frontages to ensure that the façade presents well when viewed from the north across the rail reserve and from public spaces.
- 2.3 Where two facades are abutting, the abutting walls are not required to be detailed if they are to be constructed simultaneously.
- 2.4 Northern facades of all buildings, with the exception of the ground floor, shall be articulated to maximise natural light, views across to the town centre and amenity for building occupants.
- 2.5 Ground floors shall have a minimum 3.6m floor-to-floor height.
- 2.6 Areas of greatest activity and interest are required to be orientated toward the street front and priority pedestrian routes.
- 2.7 The proportion of glazing at street level should be increased to improve casual surveillance and strengthen the relationship with the street.



A blend of materials including steel, brick and render is permitted.



- 2.8 The selection of materials and colours is to complement the architectural aesthetic of the traditional 'Administrative' and 'Industrial' buildings of the Workshops. An anti-graffiti treatment may be required for a portion of the development.

3 ROOFS

- 3.1 Roofs of Lots 669, 500 and 499 should be pitched at 30 degrees with the axis of the ridge running east-west. Simple gables or hips may terminate the roof. Roofs may include lofts. Flat roofs behind a parapet are acceptable.
- 3.2 Roofs of Lot 502 and 503 shall be screened behind a parapet wall. Alternatively, a saw toothed roof, of a similar proportion to that of the adjacent Block 1 industrial building, is acceptable.

4 PEDESTRIAN ACCESS AND SHELTER

- 4.1 Entries shall be easily distinguishable with universal access provided. Where long ramps are required they shall be provided within the building rather than externally to reduce their visual impact and assist in achieving a strong built edge to the street boundary.
- 4.2 A minimum 2 metre wide awning for pedestrian shelter shall be provided on the ground floor on the Yelverton Drive and Centennial Place frontages. Awnings are also encouraged on side faces.

5 ACCESS & PARKING

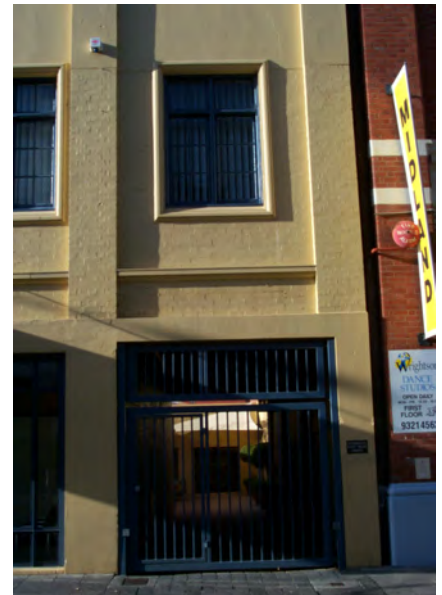
- 5.1 Vehicle access points are mandated as shown on plan in Figure 3 .
- 5.2 Lots 499 and 502 are required to share a reciprocal right of access located on Lot 502. This access point shall remain available in perpetuity for each lot.
- 5.3 Parking shall not be accommodated between the street front and the development although it may remain visible beside the development.
- 5.4 Basement parking is encouraged. Semi-undercroft parking may be supported where it appears predominantly solid and reads as though it were part of the base of the building.
- 5.5 The ground floor may be raised to a maximum of 1.2m above the public footpath level to accommodate semi-undercroft parking.
- 5.6 Where vehicle access to an enclosed (basement or semi-undercroft) parking area is provided the opening shall be a minimum height of 2.85m to allow potential for on site rubbish collection.



Roofs should be pitched at 30 degrees



A minimum 2m wide pedestrian awning shall be provided at street level



Vehicle access shall be integrated as part of the wall of the building.

6 FENCING

- 6.1 Solid walling as fencing, especially to the rail reserve, is discouraged as it is vulnerable to graffiti and limits passive surveillance and solar access. An open metal railing style of fencing is sought.
- 6.2 Any fencing to other than the rail reserve shall be a maximum height of 1.2m and be constructed of open metal railing. Fencing to the rail reserve shall be a minimum 1.8m high.
- 6.3 The preferred fence colour for metal railing is black as it visually recedes and assists in reducing the dominance of fencing.

7 SIGNAGE

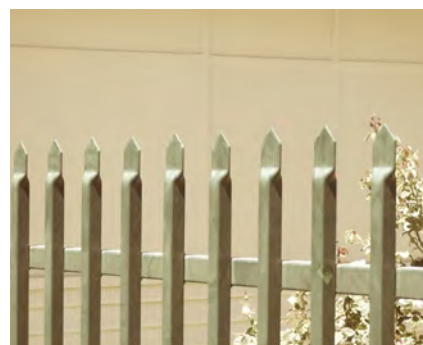
- 7.1 Signage shall be integrated with the building design and be limited in extent. Signage shall not adversely impact on visual amenity or the heritage values of the setting.
- 7.2 Preference will be given to signage that projects from the facade and is designed as an artistic element.
- 7.3 Encouragement will be given to signs that incorporate both traditional materials and new technology.

8 PUBLIC ART

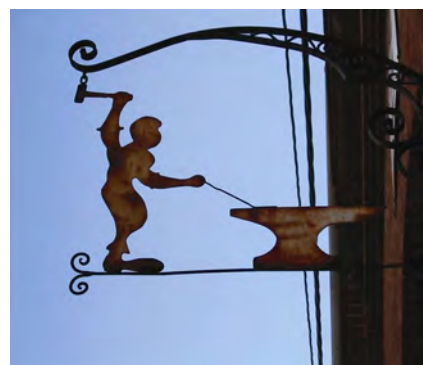
- 8.1 The Authority encourages inclusion of an artist in the building design process. Areas to consider inclusion of public art are those that are most publicly accessible, such as signage, lighting and foyer design and detailing.

9 NOISE AND VIBRATION

- 9.1 Rail and aircraft noise and rail vibration impact on The Sidings therefore a noise and vibration assessment is required as part of the development approval process. The site is also impacted by vehicle noise from Yelverton Drive. All acoustic assessment and reporting is required to be carried out by a suitably qualified and experienced professional. The recommendations of the acoustic consultant shall be incorporated within the working drawing documentation at building licence stage and shall be implemented prior to occupation. Typical design requirements are for double glazing or laminated glass; solid core entry doors; and appropriate sealing of junctions and joints to prevent air gaps.



Fencing shall be constructed of open metal railing



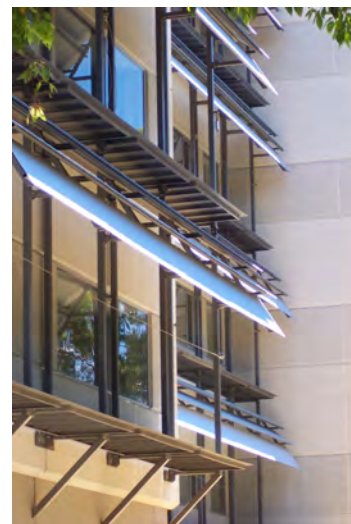
Signage designed as an artistic element that complements the Workshops history



Draw upon historical references as part of

10 ECOLOGICALLY SUSTAINABLE DESIGN

- 10.1 The Authority encourages development to be designed with;
- Energy saving initiatives;
 - Water saving initiatives;
 - Passive solar design principles; and
 - Efficient thermal performance of the building.
- 10.2 Shade awnings are to be provided to all windows and may project into the road reserve or public open space. Awnings are not permitted to project into the rail reserve.
- 10.3 A commitment to achieve a minimum 4 Star Green Star rating under the Green Building Council of Australia system is highly encouraged. The Green Building Council of Australia, Green Star rating system is explained online at (<http://www.gbca.org.au>).
- 10.4 Where Photovoltaic (PV) solar panels are proposed, a minimum of 1kW should be provided with capacity to be added to over time.
- 10.5 PV solar panels may be on public view provided they are integrated with the building design eg PVs arranged to sit flat on slope of roof; awnings may be made of PV panels).



Shade awnings shall be provided to all windows



MIDLAND
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AUTHORITY



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